

# Downtown Salina Block Guide



Growth and increased activity in downtown Salina prompted the City to develop **Downtown Arts and Entertainment (ACE) District** policies to manage various activities. These policies address food trucks, sidewalk vending, street performers, minor sidewalk dining and merchandising, pedicabs, and horse-drawn vehicles.

The goal of these policies is to protect the health and safety of people as they experience downtown. These policies also help ensuring traditional bricks-and-mortar businesses are allowed to use the public right-of-way to expand their operating space—doing so without encroachment from competing vendors.

Even on recently-rebuilt sections of Santa Fe Avenue, sidewalk space is limited in terms of what activities can occur without compromising mobility and access for people walking downtown, especially those with disabilities.

This Downtown Salina Block Guide provides illustrations for common street conditions within the Downtown Entertainment District. It is intended to provide for the safe use of public right-of-way by those interested in using sidewalk space and other public places for their business operations—whether its an expansion of an existing business or a temporary use, such as a street performer or food truck.

The Downtown Arts & Entertainment District policies strictly prohibit business activities from blocking access to crosswalks, curb ramps, pedestrian push buttons, accessible parking spaces, bike racks, and public benches. Not only are these common courtesies, blocking access to such facilities can violate the Americans with Disabilities Act (ADA).



## ACE DISTRICT

DOWNTOWN SALINA - ARTS, COMMERCE AND ENTERTAINMENT DISTRICT



While ADA stipulates a pedestrian route must be at least 4-feet in width, Downtown Salina requires at least 6-feet of clear space on a sidewalk. Nearly one in five Salina residents have a disability, which means they may require additional space when using a sidewalk. Parents using a stroller or an older adult using a cane or walker needs additional sidewalk space to safely move about.

Six-feet of space allows enough room for two people to walk side-by-side and socialize, as walking is by its very nature a social activity. That space also allows two people to pass each other in opposite directions while on the sidewalk.

This is why business owners and those performing activities allowed under the Downtown Arts and Entertainment District ordinance must be mindful of what they place in the sidewalk space. The simple act of putting a sandwich board sign in the sidewalk can be a barrier to access for people with disabilities. That is why these signs and other sidewalk merchandising must be placed in the buffer between the sidewalk and street (if a buffer exists).

Downtown Salina’s plazas were designed to connect people using parking lots on the back side of building fronting Santa Fe Avenue. Like sidewalks, space must be clear through the plazas so people can safely access them. With more businesses fronting the plazas and requesting outdoor dining space, it is desirable that a width of at least 6-feet be kept clear. Wider walking aisles are desired and are addressed on a case-by-case basis.

Other ways in which businesses and others using the sidewalk and plazas can help ensure access is maintained for people using them are:

- Training employees on proper placement of signs and other outdoor furnishings.
- Checking with City staff on best placement of items in the sidewalk space.
- Ensuring clear access for people with disability to get to the sidewalk from accessible parking spaces.
- Working with street performers and other sidewalk vendors to help them understand the needs of sidewalk users.

## PERMIT FEE GUIDE

Food Trucks.....	\$50
Horse Drawn Carriages.....	\$50
Sidewalk Vending.....	\$25
Street Performers.....	\$25
Outdoor Merchandise.....	\$50
Minor Sidewalk Dining.....	\$50

Permit is good through the calendar year. Obtain the permit online at:

<https://salina.onlama.com/Apply.aspx>

Space required for two people using a sidewalk.



Poor placement of signs can restrict access for wheelchair users.



## Downtown Arts & Entertainment District

The District is comprised of the area outlined in blue in the map at right. Santa Fe Avenue and Iron Avenue are the major cross streets as the District spans from Elm Street on the north to South Street. From east to west, the District is situated between 9th Street Front Street.

The Downtown Arts & Entertainment District Ordinance addresses how temporary uses and fixtures can be allowed to promote economic development, spur street life, and utilize the sidewalk and buffer space, as well as plazas, within the District.

The design of each block in the District varies greatly with the exception of Santa Fe Avenue. Santa Fe Avenue underwent a \$12 million renovation that was completed in 2019. This project brought with it a consistent design to Santa Fe Avenue. The project created wider sidewalk space and consistent buffers between the sidewalk and parking to allow for several uses. Permanent fixtures include benches, planters, art pedestals, bike racks, and trash receptacles.

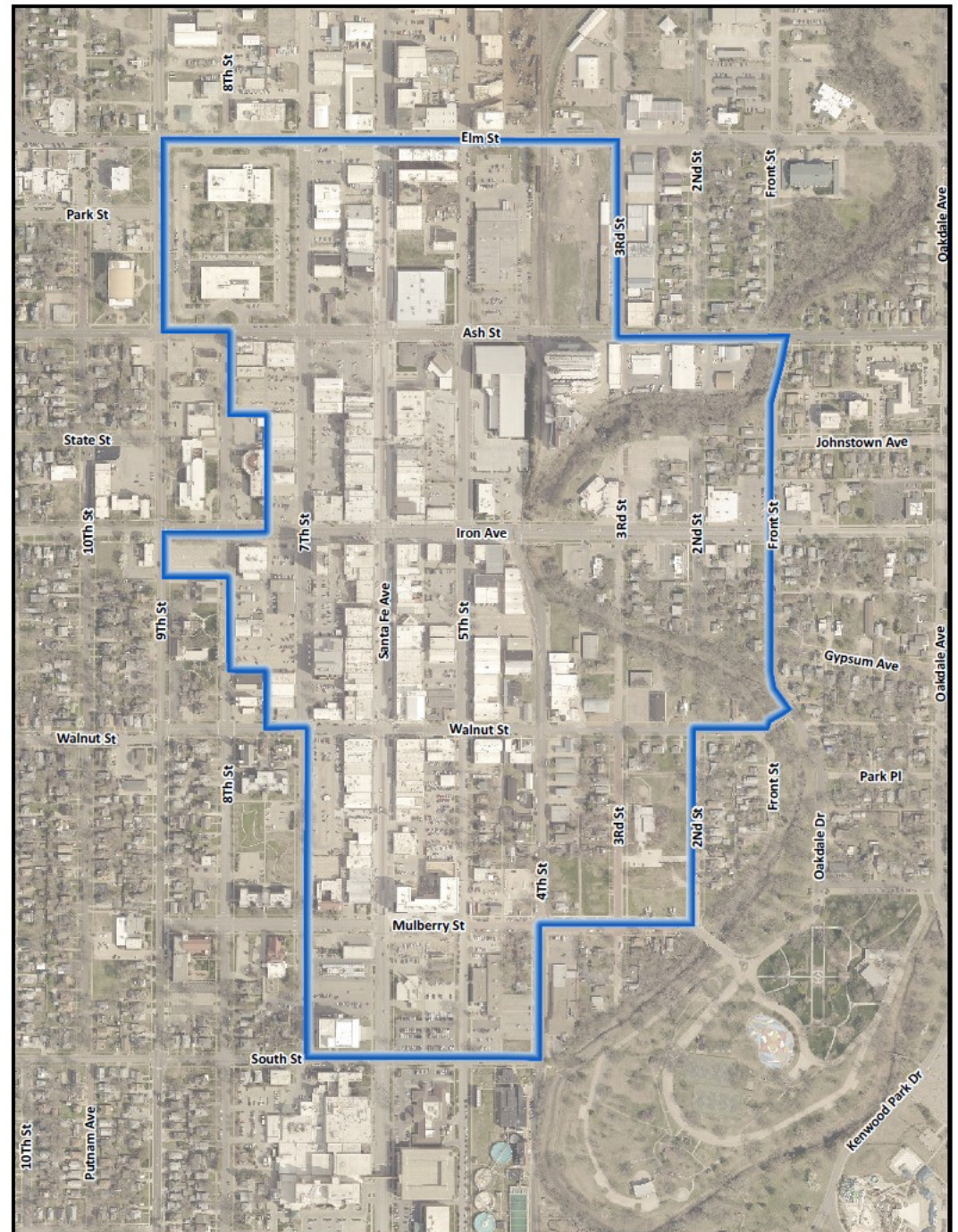
Other blocks within the District may require more site specific analysis by business owners and applicants looking to do temporary uses. This is because the existing sidewalk space may not be wide enough to accommodate the same uses that easier to implement along Santa Fe Avenue, as noted in the previous section on pedestrian circulation and access routes.

Some blocks within the District have landscaped buffers between the sidewalk and street that could be used for certain entertainment and vending users, providing access is maintained for people with disabilities to access services provided in the buffer space. For example, a table set-up to do food tasting or provide outdoor coffee service should be placed so someone can access it from the sidewalk.

The City will consider applications for altering buffer space on streets other than Santa Fe where modifications will help a business or property owner. These modifications are handled on a case-by-case basis and may include:

- Paving all or a portion of a landscaped buffer.
- Removing old planter boxes to create a level space.
- Relocating bike racks, benches, trash receptacles, traffic signs, and other fixtures that are not tied to utilities (e.g. hydrants, light poles).

Traffic signs that serve a location-specific control function (e.g. no parking zone signs) may not be considered for relocation.

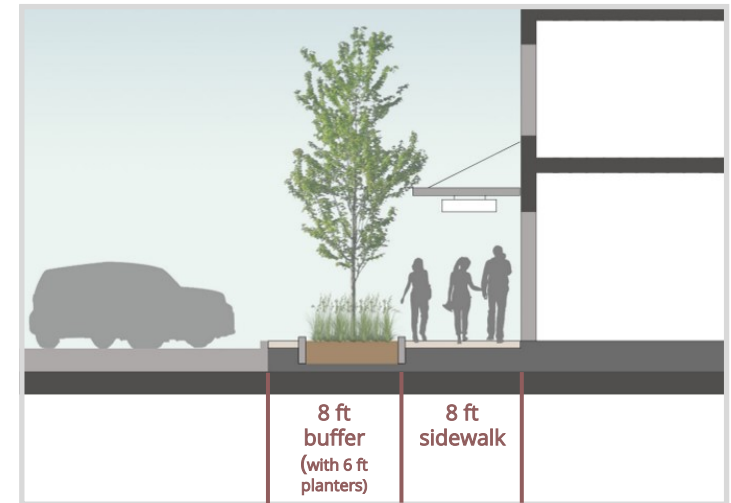


### Santa Fe Avenue, Elm Street to South Street

Santa Fe Avenue from Elm to Mulberry has concrete sidewalk widths of 8 feet (with minor exceptions) when measured from the front of buildings to the edge of the brick sidewalk buffer. As the diagram at right shows, this provides ample space for pedestrians to move about Santa Fe Avenue. The 8-foot wide buffers provide space for a variety of retail and entertainment uses except when things like art pedestals and tree planters are in the buffer.

Many intersections along Santa Fe Avenue were designed with corner nodes to enhance the streetscape with landscaping and small plazas for people to dwell. Many of these corner nodes provide space for street performers, sidewalk vending, and other uses providing they do not block access to pedestrian push buttons, curb ramps, benches, and bike racks. Due to variations in size, those wishing to use these corner nodes for uses identified in the Downtown Arts & Entertainment District Ordinance should refer to the brochures for the desired use to ensure there is enough space to operate in the node. Corner closest to the street may be restricted for some uses in order to provide appropriate sight distance for drivers approaching the intersections.

Typical Section along Santa Fe Avenue



### Other Downtown Blocks

Beyond Santa Fe Avenue there are additional opportunities for merchandising, vending, minor sidewalk dining, food trucks, and other uses. The design of these streets is not as consistent as it now is with Santa Fe Avenue. Land uses and activities generated by them vary greatly, sometimes from one block to another.

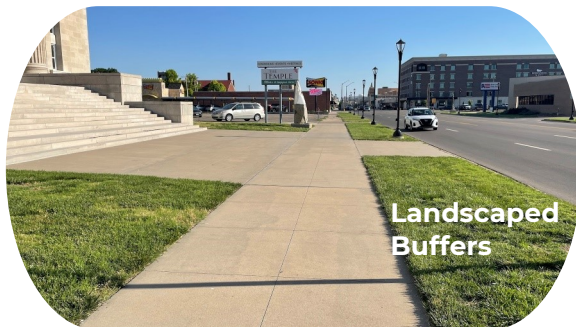
The map at right shows which streets are most, somewhat and least suitable for uses identified in the Downtown Arts and Entertainment District ordinance. Because a block is labeled as least suitable does not mean these uses are prohibited, as special events may occur along them that could create demand for such uses.

As redevelopment occurs along the streets, the City may consider features that are consistent with Santa Fe Avenue to help encourage creative uses along these blocks. This could include paving of landscaped buffers or removal of old tree wells and other streetscape features that have outlived their intended life.

#### Opportunities for Reconfiguration

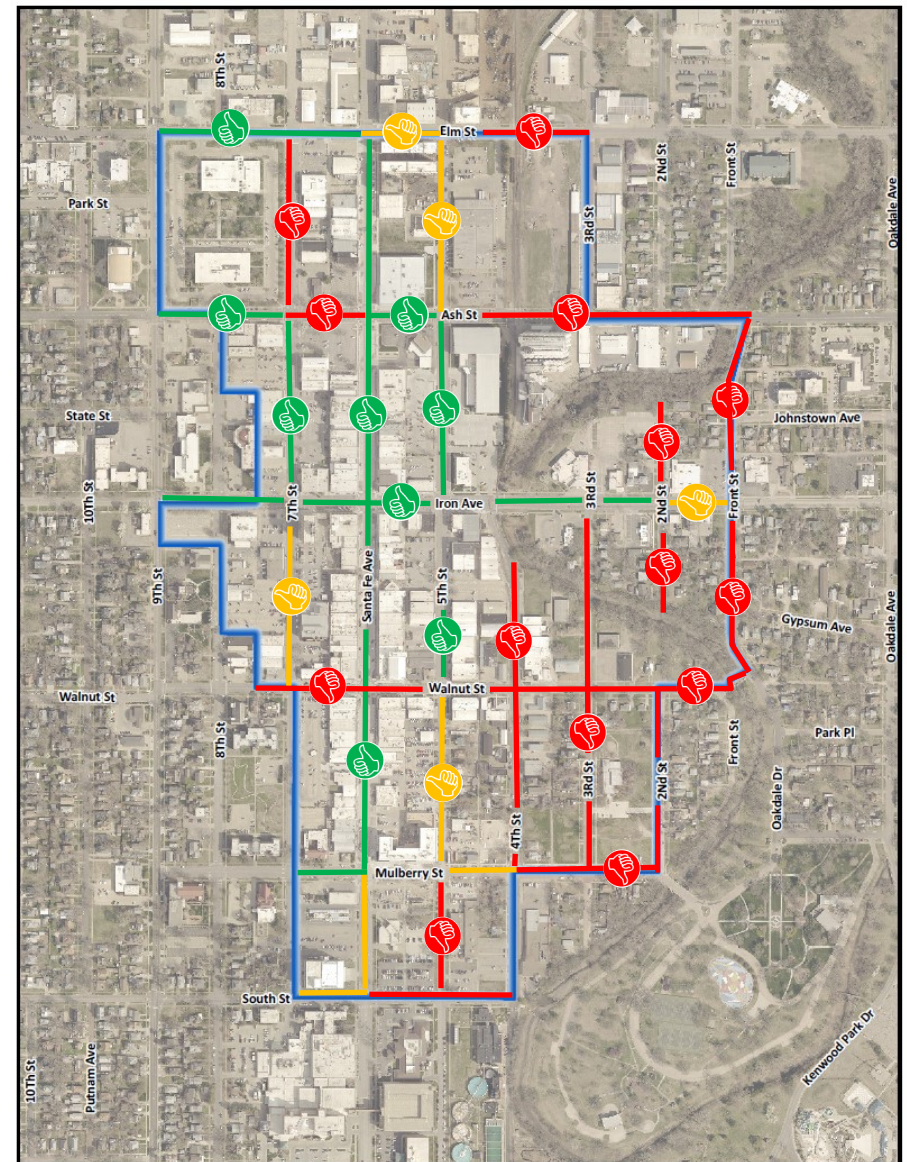





Old Planter Boxes



Landscaped Buffers

### Suitability of Downtown Blocks for Arts & Entertainment Uses



-  Most Suitable
-  Somewhat Suitable
-  Least Suitable






### Primary North-South Streets

Both 5th Avenue and 7th Avenue serve public parking lots on the east and west side of businesses along Santa Fe Avenue. These parking lots are connected to Santa Fe Avenue via public plazas and other walkways. Uses such as food trucks and pushcarts are most suitable along 5th Avenue and 7th, with spots available for food trucks near some parking lot entries to the public plazas.




A mix of buffer treatments—paved and grass—also impacts the type of uses that can occur in front of businesses. Paved buffers may contain many more uses and the City may consider allowing new businesses to pave landscaped buffers or remove old tree planters to make room for use of this space for expanded business opportunities.

New businesses along blocks with somewhat or least suitable conditions may consider integrating sidewalk vending and entertainment uses into their business and site plans to help activate these streets.

#### 5th Ave, Elm St to South St

Section	Suitability	Buffer	On-Street Parking	Notes
Elm to Ash		✓	Parallel	Southern half of block is more suitable.
Ash to Iron		✓	Parallel	West side next to parking lots has most activity.
Iron to Walnut		✓	Parallel	Paved buffer on east side.
Walnut to Mulberry		✓	Parallel	Grass buffer, some residential
Mulberry to South		✓	Parallel	Limited activity.

#### 7th Ave, Elm St to Walnut St





Section	Suitability	Buffer	On-Street Parking	Notes
Elm to Ash		✓	Parallel	Limited activity.
Ash to Iron		✓	Parallel	East side next to parking lots has most activity.
Iron to Walnut		✓	Parallel	Parking lot activity.

### Secondary North-South Streets



Other north-south streets in the Downtown Arts & Entertainment District are currently not suitable for vending and entertainment uses. This is due to a lack of commercial activity, as existing commercial uses are not conducive to generating pedestrian traffic. Other secondary north-south streets have residential uses.

New development that emerges in this area could be suitable for sidewalk-based uses related to the business, such as minor sidewalk dining and merchandising.



#### Front Ave, Walnut St to Ash St

Section	Suitability	Buffer	On-Street Parking	Notes
Walnut to Iron			Parallel	Residential area.
Iron to Ash			Varies	Limited activity.



#### 2nd Ave, Mulberry St to Iron Ave

Section	Suitability	Buffer	On-Street Parking	Notes
Mulberry to Iron			Parallel	Dead end, residential streets.

#### 3rd Ave, Mulberry St to Iron Ave

Section	Suitability	Buffer	On-Street Parking	Notes
Mulberry to Iron			Parallel	Residential area.

#### 4th Ave, Mulberry St to Iron Ave

Section	Suitability	Buffer	On-Street Parking	Notes
Mulberry to Iron			Parallel	Limited activity & residential





### East-West Streets

East-west streets in the Downtown Arts & Entertainment District do not have the same advantages as north-south streets do in terms of access to parking. Blocks closer to Santa Fe Avenue, especially intersections with Santa Fe, tend to have higher suitability and may transition in use due to this proximity.






Streets that lack buffers between the sidewalk and street may be most challenging to incorporate vending and merchandising uses. As properties redevelop the City may consider requiring buffers be added or spaces provided behind the sidewalk for additional merchandising, vending, arts, and entertainment uses.

New businesses along these blocks with moderate or least suitability along primary north-south streets may consider integrating sidewalk vending and entertainment uses into their business and site plans to help activate these streets.

### Elm St, 3rd Ave to 9th Ave

Section	Suitability	Buffer	On-Street Parking	Notes
3rd to 5th		✗	Varies	Limited or no sidewalk.
5th to Santa Fe		✗	Mixed	Constrained space.
Santa Fe to 7th		✓	Parallel	Mostly grass buffer.
7th to 9th		✓	No	Buffer on north side.

### Ash St, 4th Ave to 7th Ave

Section	Suitability	Buffer	On-Street Parking	Notes
Front to RR Track		✓	No	Limited activity.
RR Track to 5th		✗	Diagonal (south side)	Limited activity.
5th to Santa Fe		✓	Diagonal (south side)	Buffer on north side.
Santa Fe to 7th		✗	No	Buffer on north side.
7th to 9th		✓	Parallel	Buffer on south side.



### Iron Ave, Front St to 9th Ave

Section	Suitability	Buffer	On-Street Parking	Notes
Front to 2nd		✓	Limited	Some residential uses.
2nd to 5th		✓	Mixed	Grass buffer.
5th to 9th		✓	Mixed	Paved buffer east of 8th.



### Walnut St, Front St to 7th Ave

Section	Suitability	Buffer	On-Street Parking	Notes
Front to 4th		✓	Parallel	Residential, limited activity.
4th to Santa Fe		✓	Parallel	Mostly grass buffer.
Santa Fe to 7th		✓	Parallel	Paved buffer.

### Mulberry St, 4th Ave to 7th Ave

Section	Suitability	Buffer	On-Street Parking	Notes
4th to 5th		✓	Parallel	Transitioning area.
5th to 7th		✓	Varies	Transitioning area.



### South St, 4th Ave to 7th Ave

Section	Suitability	Buffer	On-Street Parking	Notes
4th to 5th		✓	Parallel	Limited activity.
5th to Santa Fe		✓	None	Grass buffer.
Santa Fe to 7th		✓	None	Special events (?)

## Pedestrian Circulation Route or Path

ADA guidance for public right of way defines the pedestrian circulation route or path (PCR) as:

- The prepared exterior or interior surface provided for pedestrian travel in the public right-of-way.

The importance of understanding the PCR is to keep the area that is fully prepared for pedestrian use clear of things such as horizontal obstructions that may not be detectable to a people with disabilities, especially those with vision disabilities.

The diagram at right shows that the PCR may be wider than the defined sidewalk as a person may use the area shaded in blue to walk or access the benches.

Objects along or overhanging a PCR are not recommended unless they can be detected by people with vision disabilities walking with the aid of a cane. Features like the lamp posts and benches have edges facing the PCR that are detectable to people with vision disabilities.

Any object placed at a height between 27 inches and 80 inches cannot protrude horizontally more than 4 inches. In a downtown environment, objects that protrude into the PCR are typically post-mounted signs.

Portions of a PCR may exceed the ADA requirement for a 2% cross slope (or less) provided there is a Pedestrian Access Route (next section) of at least four-feet in width with a cross slope no greater than 2%

## Pedestrian Access Routes

The Pedestrian Access Route (PAR) is a continuous and unobstructed path of travel provided for pedestrians with disabilities within or coinciding with a pedestrian circulation path. The City of Salina has defined a six-foot wide PAR as the preferred width in downtown to not just comply with ADA but provide space for higher volumes of pedestrians.

As noted above, the PAR must be a minimum width of 4-feet, providing a 5-foot wide space is present at least every 200 feet to allow for two people in wheelchairs to pass by one another. The diagram at right shows the pedestrian access route along East Iron Avenue where a PAR is maintained between the minor sidewalk dining and benches at the front of the building. The PAR requirements apply to other features like curb ramps, crosswalks, and access to accessible parking spaces.

While the PAR along Santa Fe Avenue is consistently wider than the minimum, other downtown sidewalks may be more restricted. The City may consider deviations from the six-foot width on streets with narrower sidewalks or other constraints to help businesses achieve their goals for providing outdoor retail or minor dining spaces.

Sample Pedestrian Circulation Route (PCR)



Sample Pedestrian Access Route (PAR)



## Opportunity Areas

The diagrams on the following pages represent common conditions along Santa Fe Avenue and other streets and illustrate where these uses are preferred to be placed and where they are prohibited. It is the responsibility of the permit holder to ensure all dimension and use requirements are met regardless of what these illustrations show as conditions can vary by block and change over time. As a general rule, if something is already in the buffer, it is best to provide adequate space around it for people to access and for city maintenance crews to respond to things like broken sprinklers and other maintenance needs.

### Corner Nodes

**Diagram 1** shows a typical plaza at the corner nodes. They contain features such as trash receptacles and benches and are intended for people to dwell. Entertainment and vending should be placed in the corner nearest the corner of the node where the north-south and east-west sidewalks meet. Depending on size, the use may be limited or restricted in order to maintain access to the benches.

Minor sidewalk dining and merchandising cannot be placed in these areas. The sidewalk area must remain clear and entertainment and vending uses cannot block access to the pedestrian push buttons, curb ramps, and crosswalks.

**Diagram 2** shows areas within the corner nodes located away from the intersection. Features placed in these areas include features such as light poles, public art, and bike racks.

These areas may be used for entertainment, vending, minor sidewalk dining and merchandising. These areas are preferred for minor sidewalk dining and merchandising as they do not limit the sidewalk space and allow the pedestrian access route to remain clear.

Areas highlighted in Diagrams 1 and 2 areas are considered pedestrian circulation routes and cannot contain protruding objects (see page X).





**Sidewalk Corridors**

**Diagram 3** shows a typical sidewalk buffer that contains things like light poles, art pedestals, and traffic signs.

Entertainment and vending should be placed in the buffer area and not placed upon art pedestals.

Minor sidewalk dining and merchandising can be placed in these areas providing .

The sidewalk area must remain clear and entertainment and vending uses cannot block access to businesses or plaza entries.

These buffer areas are considered pedestrian circulation routes and cannot contain protruding objects (see page X).



**Diagram 4** shows areas within the sidewalk corridor where there are accessible parking spaces. The curbs are designed to allow a person with a disability to access the sidewalk from the painted access aisle next to the accessible parking space.

The painted area, as well as the buffer space that provides access to the sidewalk is a Pedestrian Access Route and must remain clear of any uses.

These areas are considered pedestrian circulation routes and cannot contain protruding objects (see page X).

**Sidewalk Dining**

**Diagram 5** shows typical placement for sidewalk dining. Kansas law requires restaurants serving alcohol to have dining areas that are contiguous to the buildings because servers cannot carry alcoholic beverages across public right of way to an area that is not abutting the restaurant.

This is why sidewalk dining is not placed in the buffer. The placement of sidewalk dining in the sidewalk requires the pedestrian access route to shift to the buffer so people can navigate around it. The areas before and after the limits of the sidewalk dining space but also be clear for a minimum of 4-feet to allow people to return to the sidewalk.

No uses are allowed in these areas in order to maintain a clear pedestrian access route (see page X). These areas are also considered pedestrian circulation routes and cannot contain protruding objects (see page X).



**Mid-Block Nodes**

**Diagram 4** shows areas within the sidewalk corridor where there are accessible parking spaces. The curbs are designed to allow a person with a disability to access the sidewalk from the painted access aisle next to the accessible parking space.

The painted area, as well as the buffer space that provides access to the sidewalk is a Pedestrian Access Route and must remain clear of any uses.

These areas are considered pedestrian circulation routes and cannot contain protruding objects (see page X).





**Mid-Block Nodes**

Diagram 7 shows the areas around the mid-block crossings. There is limited space at these locations for entertainment and vending. It can be placed in between pillars and art pedestals and trash receptable provided the use does not block pedestrian access to the crosswalks.



The center lane of Santa Fe Avenue is meant to be an area for delivery trucks to dwell while unloading supplies for adjacent businesses. Pedicab and horse-drawn vehicle operators may dwell here to board and alight passengers near the mid-block crosswalks.

ONLY THE AREA ON EITHER SIDE OF THE CROSSWALK CAN BE USED FOR THESE PURPOSES. AREAS CLOSER TO OR ADJACENT TO INTERSECTIONS AND ALLEYS CANNOT BE USED.

### Older Street Designs

**Diagram 9** shows common features along streets with older design, primarily planter boxes or tree wells between the sidewalk and curb. These fixed obstacles may restrict the ability of a restaurant to provide sidewalk dining areas that serve alcohol but may not limit space for minor sidewalk dining and merchandising, as shown in the yellow shaded areas.

Given the age of this infrastructure, the City may consider proposals by businesses to remove planter boxes to accommodate a variety of uses.



### Curb Ramps

**Diagram 10** shows a transition from the sidewalk to a street corner curb ramp and intersection where the ramp begins along the buildings frontage. The area in red is a Pedestrian Access Route. These areas are crucial for people with disabilities to access sidewalk and curb ramps in these corner locations and should not be used for any special use.





**Older Street Designs**

Diagram 11 shows an older street design that can accommodate many uses within the Arts and Entertainment District without major reconfiguration of the sidewalk space. In this example there is ample sidewalk space between the shaded areas, which allows businesses to use these areas for vending and entertainment purposes. Sidewalk dining at a restaurant that serves alcohol may be possible, but limited to one row of tables close to the building.



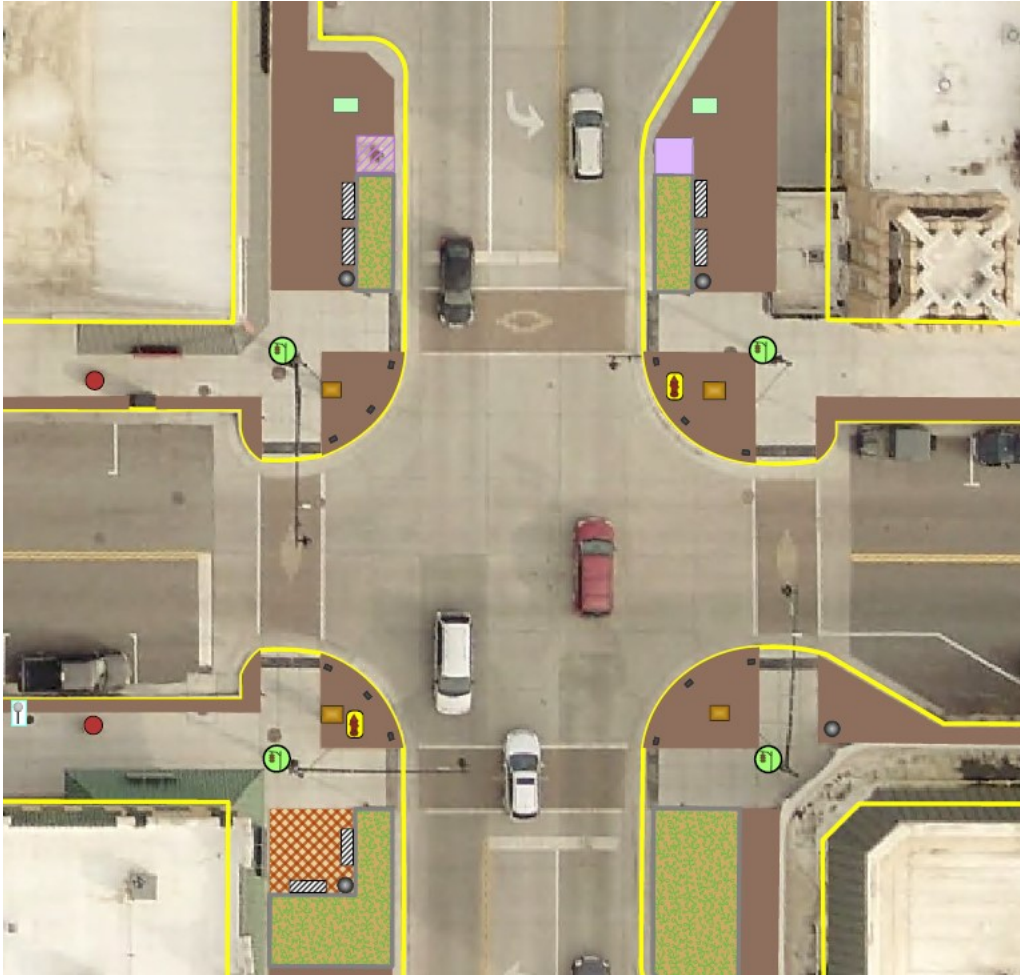
**Buffers with Steep Slopes**






Diagram 12 shows an example of recent redevelopment that required substantial reconfiguration of the space between the curb and building to provide access to the business. The result is a paved buffer that has an excessive cross slope that cannot be used by people who use mobility devices.

The excessive slopes prohibit many vending uses due to stability of equipment that must be placed there. These slopes also mean that uses adjacent to the building are not possible since the red shaded area would have to serve as a Pedestrian Access Route but is not compliant due to the cross slope toward the street.

Businesses may be other structures, such as a patio, over these slopes, to have merchandising and minor sidewalk dining areas.





-  Fire Hydrant
-  Traffic Signal
-  Decorative Street Lamp on a Post
-  Overhanging Intersection Street Light
-  Overhanging Street Lamp on Traffic Signal
-  Other

